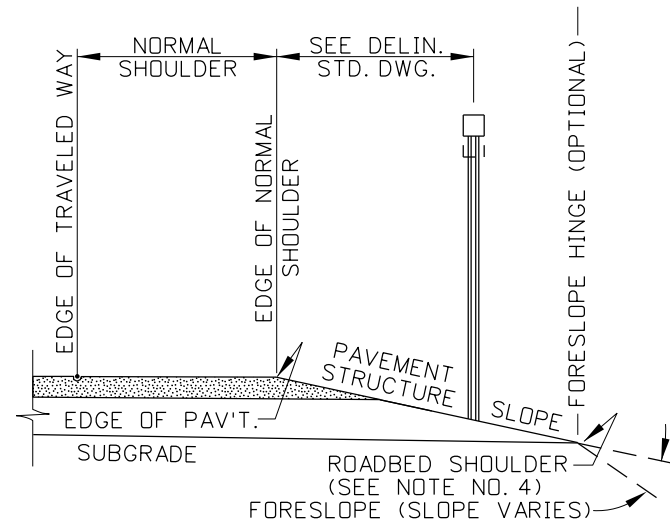
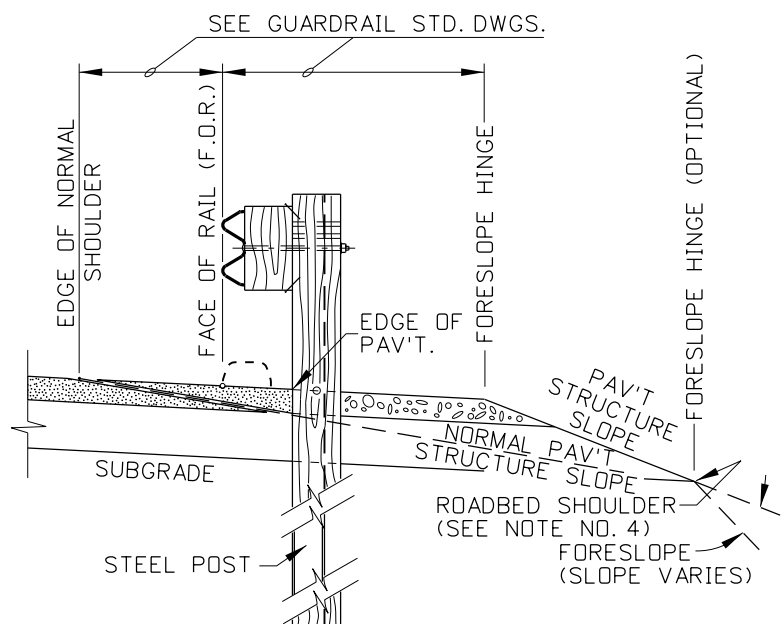


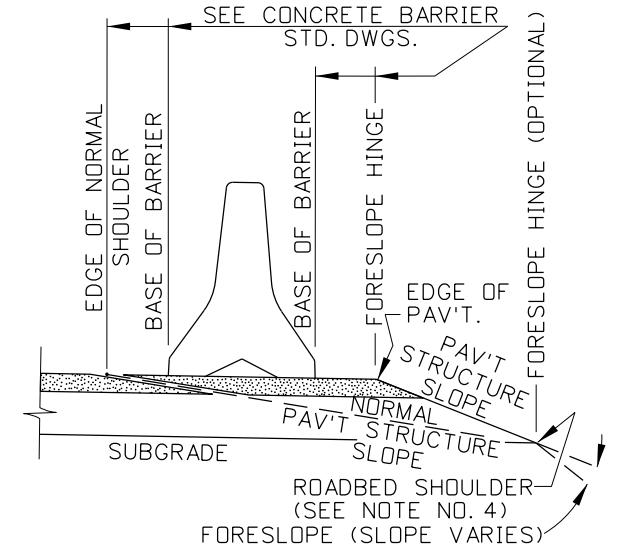
TYPICAL HIGHWAY



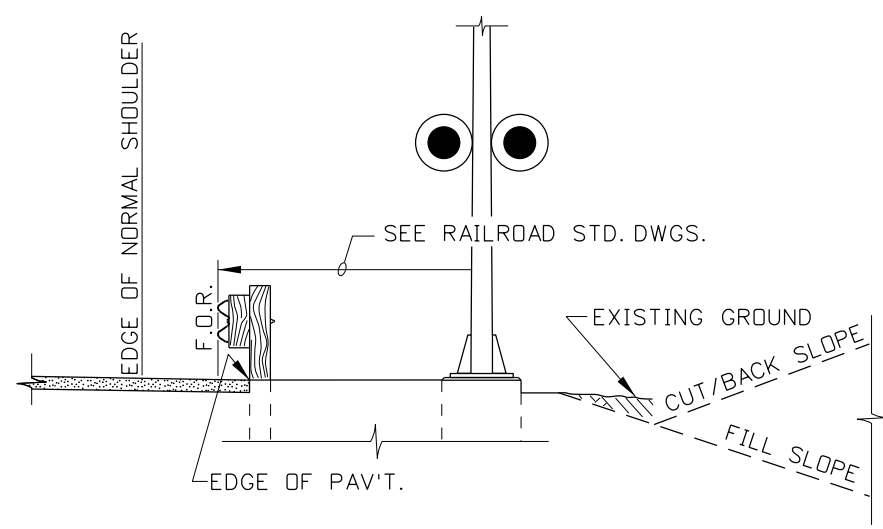
DELINEATOR INSTALLATION



STANDARD W-BEAM INSTALLATION (ASPHALT)



CONCRETE BARRIER INSTALLATION



RAILROAD INSTALLATION WITH GUARDRAIL

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	
DESIGN CHECKED	
DETAILED	
DRAWING CHECKED	

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY	
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**IDAHO TRANSPORTATION DEPARTMENT**

PROJECT NO. \_\_\_\_\_

STANDARD DETAIL A-9

**ITD ROADWAY NOMENCLATURE LOCATIONS & EXAMPLES**

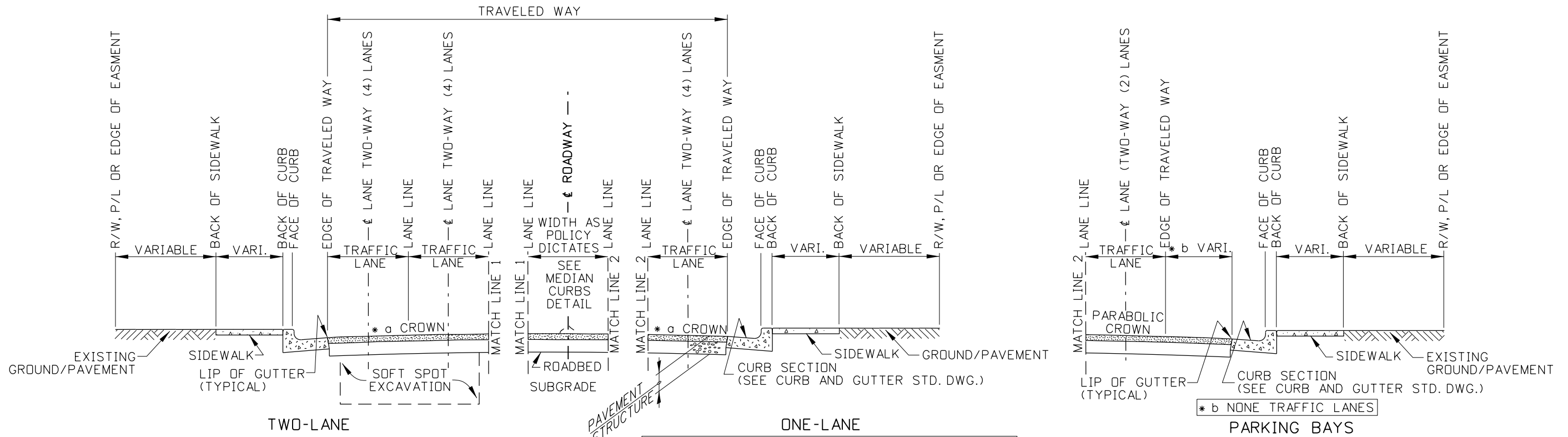
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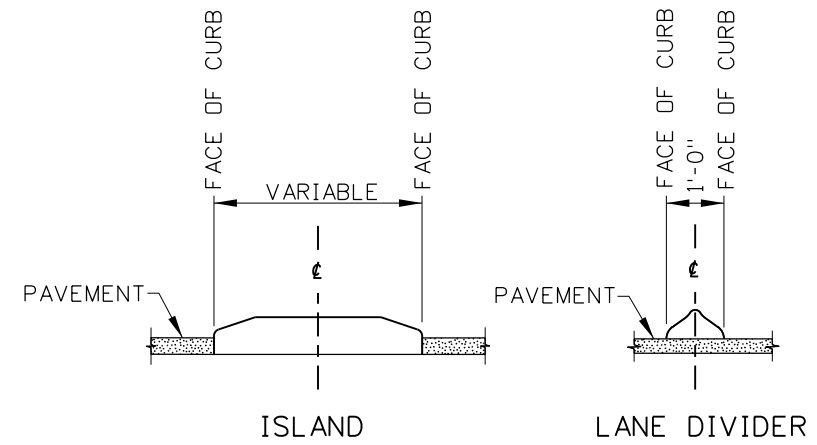
SHEET 1 OF 4

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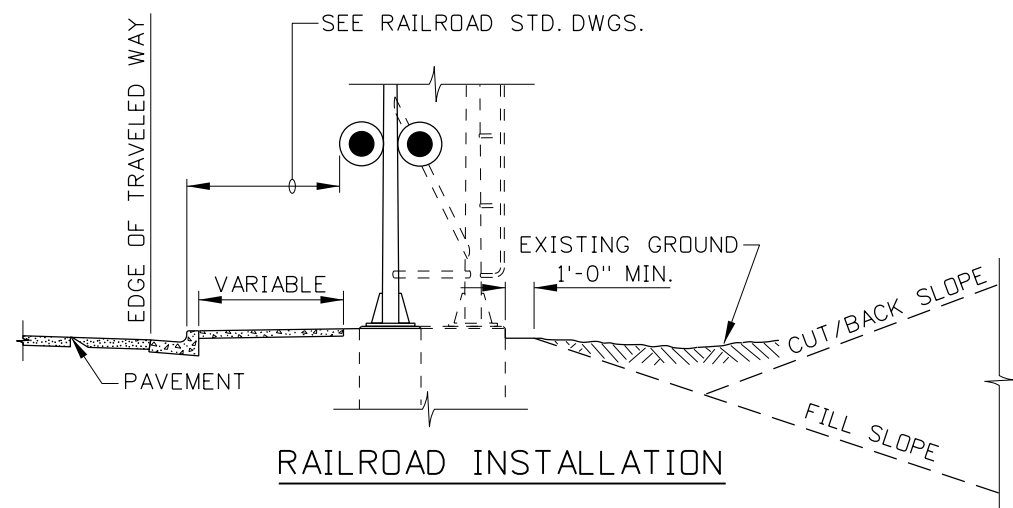


\* a A PARABOLIC CROWN IS NORMALLY INSTALLED ON URBAN ROADWAYS BETWEEN CURB & GUTTERS (SEE NOTE NO. 5)

TYPICAL URBAN STREET



MEDIAN CURBS  
(SEE CURB AND GUTTER STD. DWG.)



RAILROAD INSTALLATION

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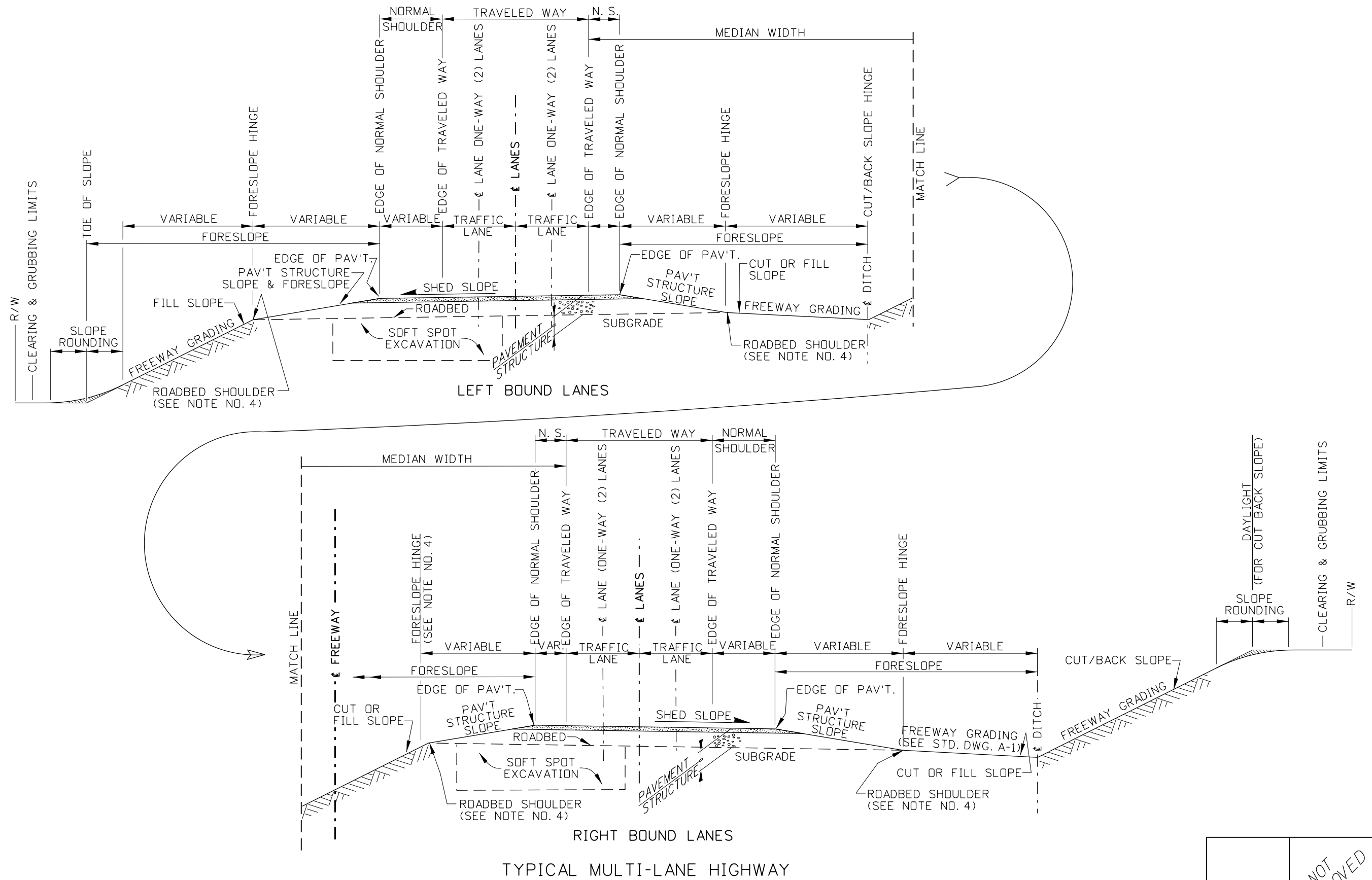
**IDAHO TRANSPORTATION DEPARTMENT**

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STANDARD DETAIL A-9  
**ITD ROADWAY NOMENCLATURE LOCATIONS & EXAMPLES**

<b>English</b>
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TYPICAL MULTI-LANE HIGHWAY

REVISIONS			
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**ITD ROADWAY NOMENCLATURE LOCATIONS & EXAMPLES**

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 SHEET 3 OF 4

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## DEFINITIONS

\* THESE TERMS ARE NOT NORMALLY USED ON STANDARD DRAWINGS.

**BACK OF CURB:** THE BEGINNING OF SIDEWALK OR UTILITY STRIP. ALSO USED FOR SURVEY CONTROL LINE.

**\*PAVEMENT STRUCTURE:** THE STRUCTURE THAT IS CONSTRUCTED ON THE ROADBED AND TYPICALLY INCLUDES SURFACING, BASE COURSES, AND GRANULAR SUBBASE.

**PAVEMENT STRUCTURE SLOPE:** THE PRIMARY PORTION OF THE FORESLOPE, BEGINNING AT THE EDGE OF PAVEMENT AND ENDING AT THE ROADBED SHOULDER.

**BASE OF BARRIER:** WHERE THE BASE OF CONCRETE BARRIER TOUCHES THE PAVEMENT (THE POINTS OF MEASUREMENT).

**\*CLEAR ZONE:** THE ROADSIDE PORTION THAT IS BEYOND THE TRAVELED WAY AND IS AVAILABLE FOR USE BY ERRANT VEHICLES.

**\*CLEARING & GRUBBING LIMITS:** AN AREA WITHIN THE ROADWAY CORRIDOR THAT ORGANIC MATTER IS REMOVED PRIOR TO PLACEMENT OF EMBANKMENT OR REMOVAL OF EXCAVATION.

**CENTERLINE (C/L):** THE CENTERLINE OF ROADWAY, TRAFFIC LANE(S), OR FREEWAY. ALSO REFERRED TO AS THE TRAFFIC MARKINGS THAT DELINEATE THE DIVISION OF OPPOSING TRAFFIC (SEE CONTROL LINE).

**CONTROL LINE:** A SURVEY LINE FROM WHICH ROADWAY DIMENSIONS ARE MEASURED (NOT NECESSARILY THE SAME AS THE ROADWAY CENTERLINE).

**CUT/BACK SLOPE:** AN ASCENDING SLOPE FROM THE EDGE OF FORESLOPE OR BOTTOM OF DITCH TO DAYLIGHT.

**EDGE OF NORMAL SHOULDER:** WHERE THE NORMAL SHOULDER ENDS.

**EDGE OF PAVEMENT:** THE EDGE OF THE TRAVELABLE PAVEMENT, WHERE THE PAVEMENT STRUCTURE SLOPE BREAKS DOWN FROM THE ROADWAY WIDTH.

**FACE OF RAIL (F.O.R.):** A VERTICAL LINE ALONG THE INNER MOST PART OF METAL GUARDRAIL THAT FACES THE ROADWAY.

**FILL SLOPE:** A DESCENDING SLOPE OF COMPACTED MATERIAL FROM THE EDGE OF ROADBED TO TOE OF SLOPE.

**FORESLOPE:** ANY DESCENDING SLOPE OR COMBINATION OF SLOPES FROM THE EDGE OF PAVEMENT TO THE BEGINNING OF A CUT/BACK SLOPE, BOTTOM OF DITCH, OR THE TOE OF SLOPE OF AN ADJACENT ROADWAY.

**HIGHWAY:** THE ENTIRE RIGHT-OF-WAY.

**HINGE (POINT):** A BREAKING POINT OF THE ROADWAY CROWN, PARABOLIC CROWN, PAVEMENT STRUCTURE SLOPE, FORESLOPE, FILL SLOPE, OR CUT SLOPE.

**LANE LINE:** EDGE OF A TRAFFIC LANE USUALLY DELINEATED BY A TRAFFIC MARKING LINE.

**LIP OF GUTTER (L.O.G.):** THE END OF THE CURB/GUTTER SECTION AND BEGINNING OF THE ROADWAY PAVEMENT. NORMALLY, THE CONTROL LINE WHEN A PARABOLIC CROWN IS INSTALLED.

**MEDIAN:** THE PORTION OF A DIVIDED HIGHWAY OR FREEWAY THAT SEPARATES THE TRAVELED WAYS FOR TRAFFIC IN OPPOSITE DIRECTIONS.

**\*MEDIAN WIDTH:** THE WIDTH OF THE AREA BETWEEN THE TRAVELED WAYS OF TWO ROADWAYS.

**NORMAL SHOULDER:** THAT PORTION OF THE PAVED ROADWAY SURFACE OUTSIDE OF THE TRAVELED WAY.

**PARABOLIC CROWN:** A CROSS-SECTION FINISH GRADE THAT CONTAINS A PARABOLIC CURVE BETWEEN CURB & GUTTERS.

**PLANS:** APPROVED DRAWINGS OR REPRODUCTION OF APPROVED DRAWINGS THAT THE PROPOSED ROADWAY IS TO BE LET FOR CONTRACT AND CONSTRUCTED.

**\*PROFILE GRADE:** A SERIES OF TANGENT GRADE LINES CONNECTED BY VERTICAL CURVES. IT IS TYPICALLY PLACED ALONG THE ROADWAY CENTERLINE OF UNDIVIDED FACILITIES AND AT THE RIGHT/LEFT LIP OF GUTTER FOR PARABOLIC CONTROL IN URBAN AREAS.

**\*ROADSIDE:** THE AREA ADJOINING THE OUTER EDGE OF THE ROADWAY WITHIN THE RIGHT-OF-WAY. AREAS (ALSO CALLED MEDIAN) BETWEEN THE ROADWAYS OF A DIVIDED HIGHWAY SHALL ALSO BE CONSIDERED ROADSIDE.

**ROADWAY BALLAST:** COMBINED PAVEMENT STRUCTURE AND EMBANKMENT (FILL) MATERIAL, INCLUDING SHOULDER MATERIAL OUTSIDE THE ROADWAY PRISM.

**\*ROADWAY CORRIDOR:** THAT PORTION OF THE HIGHWAY WITHIN THE LIMITS OF CONSTRUCTION.

**ROADWAY:** SEE ROADWAY CORRIDOR.

**ROADWAY CROWN:** A CROSS-SECTION FINISH GRADE THAT CONTAINS A PERCENT GRADE OR SLOPE (SHOWN ON THE TYPICAL SECTION).

**\*ROADWAY PRISM:** THE ENGINEERED/STRUCTURAL PORTION OF THE HIGHWAY. INCLUDES THE PAVEMENT STRUCTURE PLUS THE AREA BETWEEN THE ROADBED SHOULDERS, OR BACK OF CURB, EXTENDING DOWNWARD AND OUTWARD AT THE SLOPE OF 1.5 H TO 1.0 V TO THE INTERCEPT OF NATURAL GROUND, REMOVAL LIMIT, OR SLOPE OF EMBANKMENT KEYING BENCHES. INCLUDED ELEMENTS ARE ROADWAY PAVEMENT STRUCTURE, EMBANKMENT FILL, FOUNDATIONS FOR EMBANKMENT, AND SOFT SPOT EXCAVATION/BACKFILL. EMBANKMENT FILL OUTSIDE OF THE 1.5\*H TO 1.0\*V SLOPE IS NOT CONSIDERED PART OF THE ROADWAY PRISM (SEE DETAIL).

**\*ROADWAY SHOULDER:** ANY TRAVELABLE PORTION OF THE ROADWAY OUTSIDE OF THE TRAVELED WAY.

**\*ROADWAY WIDTH:** FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.

**SHED SECTION:** A CROSS-SECTION FINISH GRADE THAT CONTAINS A SINGLE PERCENT GRADE OR SLOPE (SHOWN ON THE TYPICAL SECTION).

**SHY LINE OFFSET:** THE DISTANCE FROM THE EDGE OF THE TRAVELED WAY, BEYOND WHICH A ROADSIDE OBJECT WILL NOT BE PERCEIVED AS AN OBSTACLE AND RESULT IN A MOTORIST'S REDUCING SPEED OR CHANGING VEHICLE POSITION ON THE ROADWAY (SEE 2006 AASHTO ROADSIDE DESIGN GUIDE, TABLE 5.5)

**SHOULDER:** THE PORTION OF THE ROADWAY CONTIGUOUS WITH THE TRAVELED WAY FOR THE ACCOMMODATION OF STOPPED VEHICLES, FOR EMERGENCY USE, AND FOR LATERAL SUPPORT OF BASE AND SURFACE COURSES (SEE NORMAL SHOULDER).

**SLOPE:** THE RELATIVE STEEPNESS OF THE TERRAIN EXPRESSED AS A RATIO OR PERCENTAGE.

**SLOPE ROUNDING:** THE INTRODUCTION OF A VERTICAL CURVE BETWEEN TWO SLOPES TO MINIMIZE THE ABRUPT SLOPE CHANGE.

**ROADBED:** THE TOP OF SUBGRADE, UPON WHICH THE PAVEMENT STRUCTURE, CURBS, SIDEWALKS, MEDIAN AND OTHER INCIDENTAL FACILITIES ARE CONSTRUCTED.

**ROADBED SHOULDER:** EDGE OF ROADBED, WHERE THE BOTTOM OF THE ROADWAY PAVEMENT STRUCTURE MEETS DAY LIGHT AT THE FORESLOPE OR FILL SLOPE.

**SOFT SPOT EXCAVATION:** EXCAVATION BELOW OR BEYOND THE NORMAL ROADWAY PRISM USUALLY DUE TO SUBGRADE MATERIAL THAT WILL NOT SUPPORT A NORMAL ROADWAY BALLAST. SMALL QUANTITIES AT SPECIFIC LOCATIONS THAT ARE USUALLY NOT SHOWN ON THE PLANS.

**TOE OF SLOPE:** WHERE THE BOTTOM OF A SLOPE (USUALLY A FILL SLOPE) INTERSECTS THE NATURAL GROUND OR BOTTOM OF DITCH.

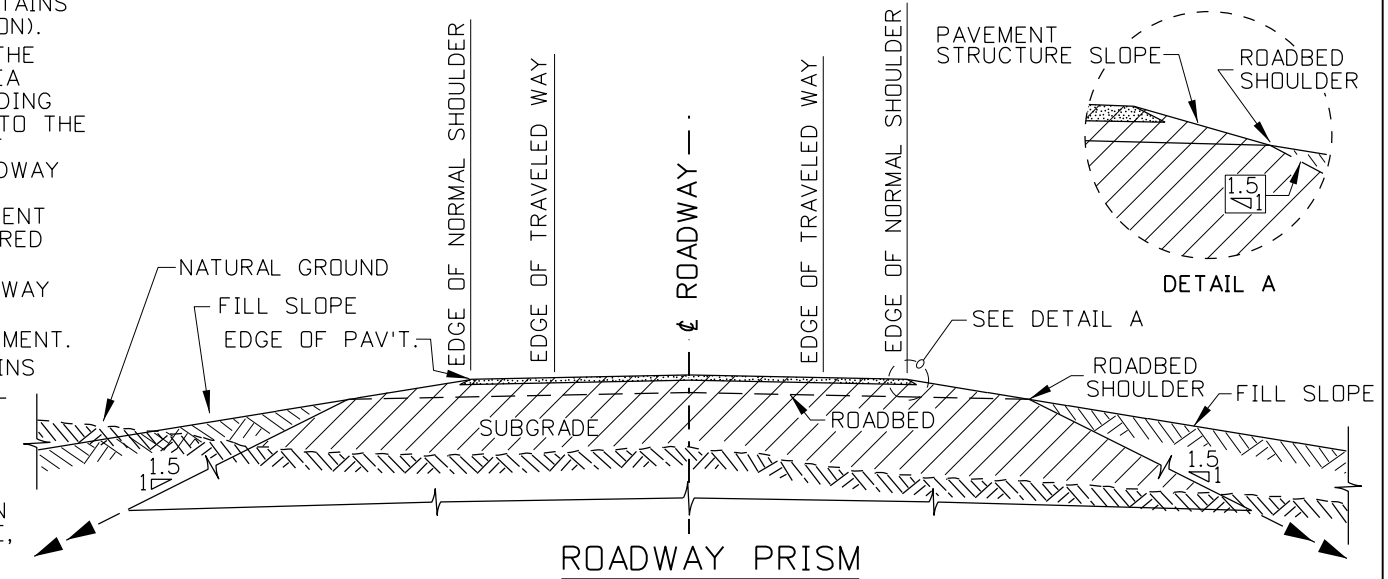
**\*TRAFFIC LANE:** THE PORTION OF THE TRAVELED WAY FOR THE MOVEMENT A SINGLE LINE OF VEHICLES.

**TRAVELED WAY:** THAT PORTION OF THE ROADWAY CORRIDOR THAT IS DESIGNATED FOR VEHICULAR TRAVEL NOT INCLUDING THE ROADWAY SHOULDERS.

**TYPICAL SECTION:** AN ELEVATION DETAIL IN THE PLANS WHICH IS A ROADWAY CROSS-SECTION THAT INCLUDES A TRAVERSE FINISH GRADE PROFILE, THE PAVEMENT STRUCTURE REQUIREMENTS AND BASIC ROADWAY CONSTRUCTION DIMENSIONS.

**\*URBAN STREET:** A PAVED STREET WITH A PARABOLIC CROWN CONNECTING CURB AND GUTTER EDGES.

THE ROADWAY PRISM IS REPRESENTED BY THE CROSS-HATCHED AREA THAT INCLUDES PAVEMENT STRUCTURE AND SUBGRADE, BUT NOT NATURAL GROUND (NOTE: TOP SOIL AND ORGANIC MATTER REMOVED).



### NOTES

1. THE ITEMS AND TERMS SHOWN ARE INTENDED TO BE GENERAL EXAMPLES AND SHALL NOT HAVE PRECEDENCE OF ANY DEFINITION CONTAINED IN THE PLANS OR STANDARD SPECIFICATIONS. SOME DEFINITIONS AND USAGE HEREIN MAY BE UNIQUE TO THE IDAHO TRANSPORTATION DEPARTMENT (ITD).
2. ADDITIONAL DEFINITION OF TERMS CAN BE FOUND IN THE AASHTO ROADSIDE DESIGN GUIDE AND THE ITD STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
3. REFER TO THE APPROPRIATE STANDARD DRAWING WHEN INSTALLING GUARDRAIL.
4. REFER TO THE APPROPRIATE STANDARD DRAWING WHEN INSTALLING STANDARD CONCRETE BARRIER.
5. REFER TO THE APPROPRIATE STANDARD DRAWING WHEN A RAILROAD CROSSING IS INVOLVED.
6. REFER TO THE APPROPRIATE STANDARD DRAWING WHEN CURB OR CURB AND GUTTER IS USED.
7. A FORESLOPE HINGE POINT IS NOT NECESSARILY AT THE EDGE OF ROADBED (SEE DEFINITION OF FORESLOPE).

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STANDARD DETAIL A-9  
**ITD ROADWAY NOMENCLATURE LOCATIONS & EXAMPLES**

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SHEET 4 OF 4

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