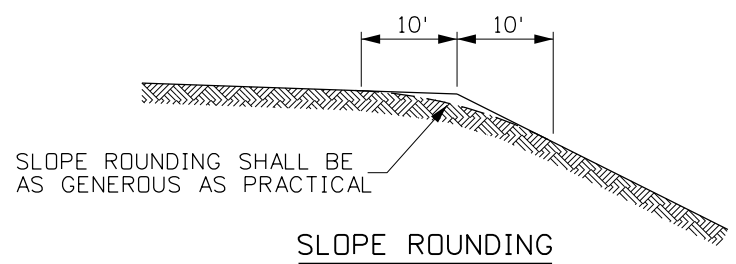
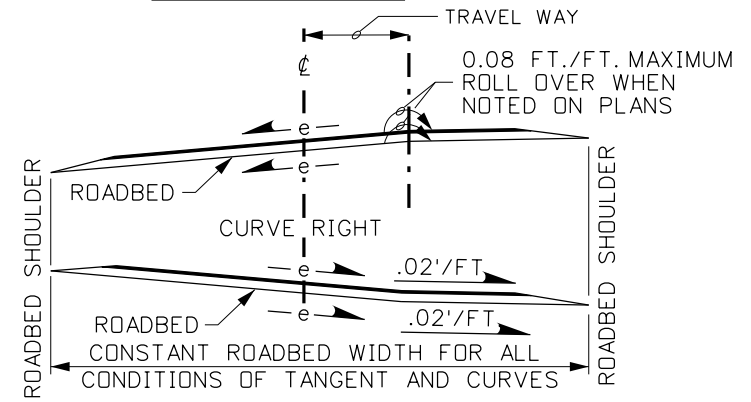


* b BERM SHALL BE CONSTRUCTED TO A HEIGHT SUFFICIENT TO PROVIDE PROPER DRAINAGE BUT NOT EXCESSIVE IN ORDER TO MAINTAIN SAFETY AND APPEARANCE.

MEDIAN BERM SECTION

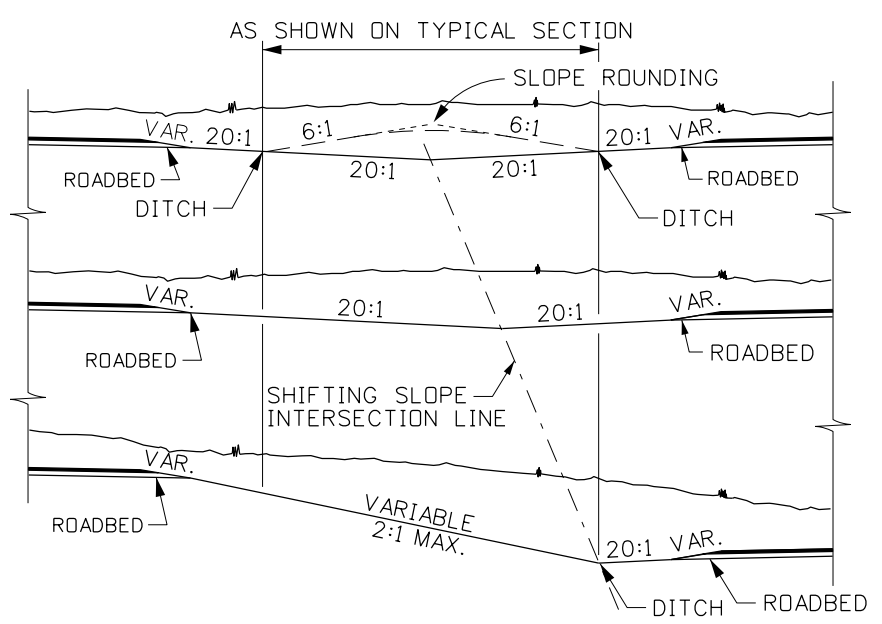


SLOPE ROUNDING



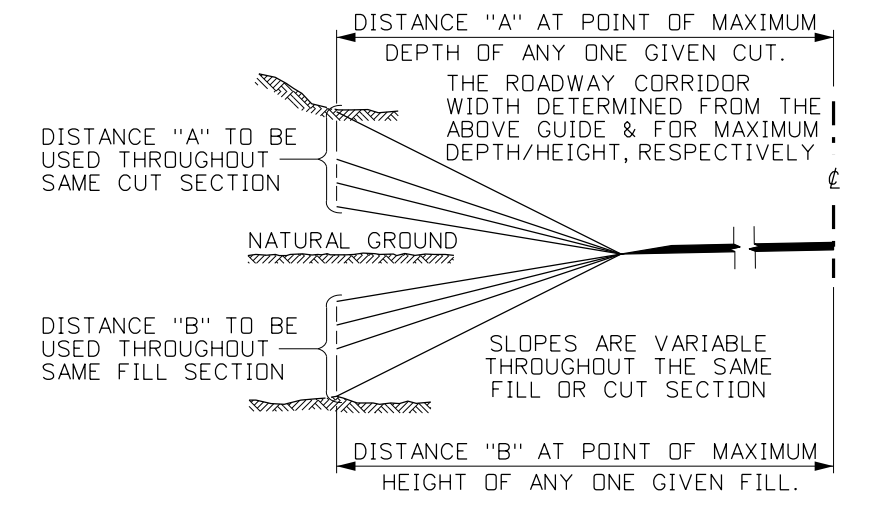
SUPERELEVATION - FOUR LANE

GUIDE FOR DETERMINING UNIFORM WIDTH



SHIFTING SLOPE MEDIAN GRADING

MEDIAN SLOPE CRITERIA	
WHERE MEDIANS ARE 100' OR LESS IN WIDTH USE A 20:1 SLOPE ON EACH ROADWAY UNTIL THE HEIGHT OF UPPER ROADWAY CAUSES THE SLOPES TO INTERSECT AT A MINIMUM DITCH DEPTH ON THE LOWER ROADWAY.	
WHEN THE UPPER AND LOWER GRADE ELEVATIONS OF SEPARATE ROADWAYS BECOME TOO GREAT TO USE A 20:1 MEDIAN SLOPE USE A VARIABLE SLOPE TO A MAX. OF 2:1 SLOPE. MAINTAIN THE MINIMUM DITCH OF THE LOWER ROADWAY.	
FOR MEDIANS OVER 100' IN WIDTH USE STANDARD INTERSTATE SLOPES. TREAT EACH ROADWAY AS A SEPARATE ROADWAY.	



SLOPE CONSTRUCTION - UNIFORM WIDTH METHOD

NOTES

- CUT AND FILL SLOPES IN DIFFICULT TERRAIN OR WHERE UNSTABLE SOIL EXISTS MAY REQUIRE SPECIAL CONSIDERATION. REFER TO THE IDAHO TRANSPORTATION DEPARTMENT'S BMP MANUAL.
- SLOPE ROUNDING SHALL CONSIST OF TWO 10' MINIMUM CHORDS OR AN EQUAL ROUNDED SURFACE.
- ROADWAY ROLL OVER IS NOT TO BE USED UNLESS NOTED ON PLANS.
- SLOPE TREATMENT SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED.
- AS STANDARD DESIGN PROCEDURE ALL SLOPES MUST BE CHECKED TO DETERMINE IF THERE IS A GUARDRAIL WARRANT BASED ON HEIGHT AND STEEPNESS OF SLOPE.
- WHEN USING GUARDRAIL, WIDEN SHOULDER AS SHOWN ON THE APPROPRIATE ITD GUARDRAIL STANDARD DRAWING.
- THE UNIFORM WIDTH METHOD FOR SLOPE CONSTRUCTION SHALL BE USED ON ITD ROADWAY PLANS WHEN PRACTICAL.
- DRAWING NOT TO SCALE.

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED	
DETAILED	CADD FILE NAME
DRAWING CHECKED	DRAWING DATE:

IDAHO TRANSPORTATION DEPARTMENT



PROJECT NO. _____

STANDARD DETAIL A-1
FREWAY GRADING

English
COUNTY _____
KEY NUMBER _____
SHEET 1 OF 1

NOT APPROVED
PRELIMINARY
FOR CONSTRUCTION