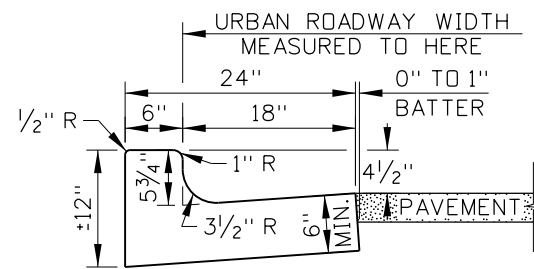
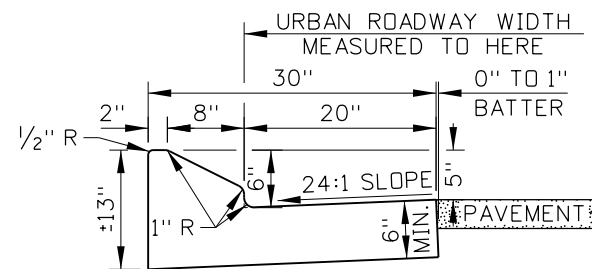


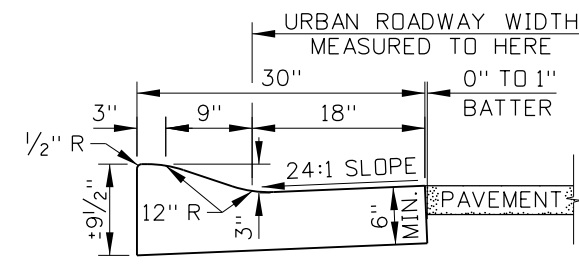
CURB AND GUTTER TYPE 1



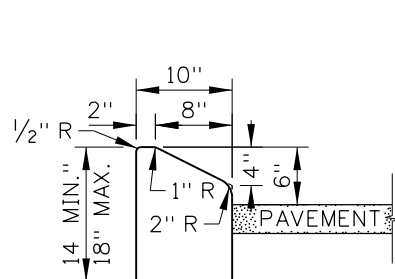
CURB AND GUTTER TYPE 2
(SEE NOTE NO. 4)



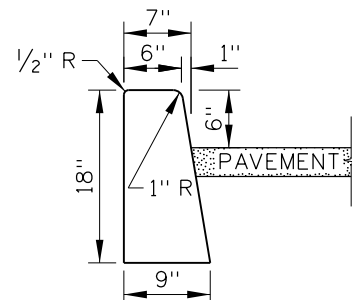
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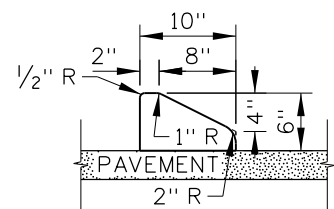
CURB AND GUTTER TYPE 4



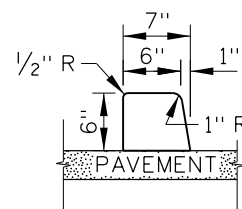
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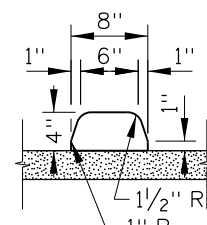
CURB TYPE 2



CURB TYPE 3
(SEE NOTE NO. 3)



CURB TYPE 4
(SEE NOTE NO. 3)



CURB TYPE 5
(SEE NOTE NO. 3)

NOTES

1. TRANSITION BETWEEN DIFFERENT TYPES OVER 10 FEET.
2. PROVIDE 4 INCHES OF AGGREGATE BASE UNDER CURB AND GUTTER, CURB, OR GUTTER UNLESS THE CURB IS PLACED ON PAVEMENT.
3. PORTLAND CEMENT CONCRETE CURB OR TRAFFIC SEPARATOR ON ASPHALT CONCRETE PAVEMENT:

PROVIDE A KEY IN THE PAVEMENT AT THE CENTERLINE OF THE CURB OR TRAFFIC SEPARATOR. SEE THE KEY DETAIL.

CONCRETE CURBS OR TRAFFIC SEPARATORS MAY BE PINNED TO THE PAVEMENT STRUCTURE IN LIEU OF THE KEY. DRILL THE PAVEMENT AND PLACE PINS BEFORE THE CURB OR TRAFFIC SEPARATOR IS CONSTRUCTED. SEE THE PIN DETAIL.

PORTLAND CEMENT CONCRETE CURB OR TRAFFIC SEPARATOR ON PORTLAND CEMENT CONCRETE PAVEMENT:

USE AN EPOXY BONDING AGENT. NO KEY IS NEEDED.

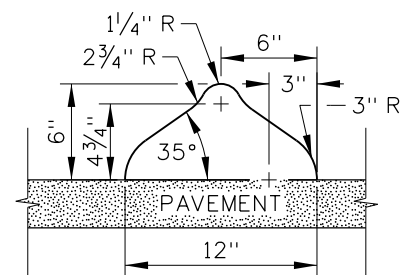
ASPHALT CONCRETE CURB OR TRAFFIC SEPARATOR ON ASPHALT CONCRETE PAVEMENT:

NO KEY IS NEEDED. ENSURE THAT THE CURB IS BONDED TO THE PAVEMENT.

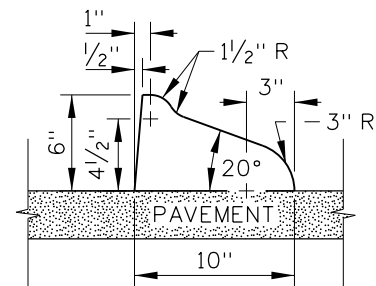
4. ENSURE THAT THE GUTTER SLOPE DOES NOT EXCEED 5 PERCENT AT CURB RAMPS.

5. TAPER THE LAST 6 FEET OF CURB AND GUTTER AND CURB TYPES 1 AND 2 DOWN TO A 1 INCH HEIGHT. TAPER CURB TYPES 3, 4, AND 5 AND TRAFFIC SEPARATORS TYPES 1 AND 2 DOWN TO A 1 INCH HEIGHT AT A 1:1 SLOPE.

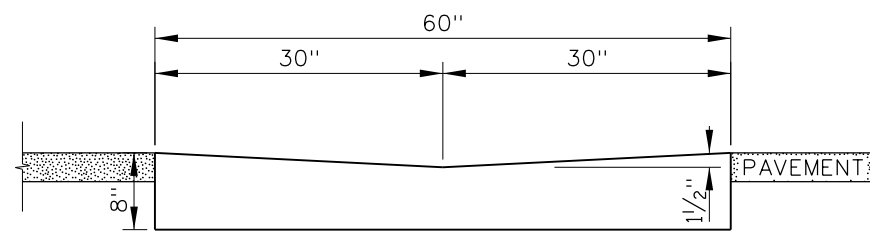
6. DRAWING NOT TO SCALE.



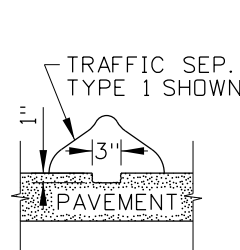
TRAFFIC SEPARATOR TYPE 1
(SEE NOTE NO. 3)



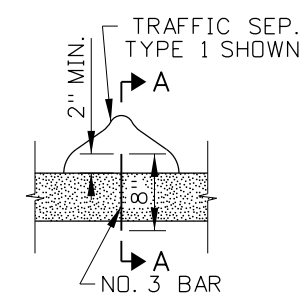
TRAFFIC SEPARATOR TYPE 2
(SEE NOTE NO. 3)



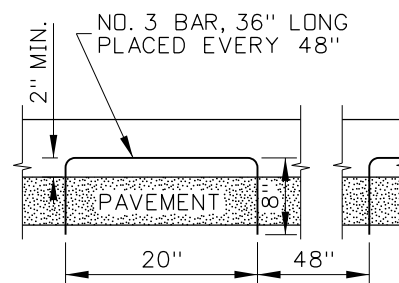
GUTTER TYPE 1



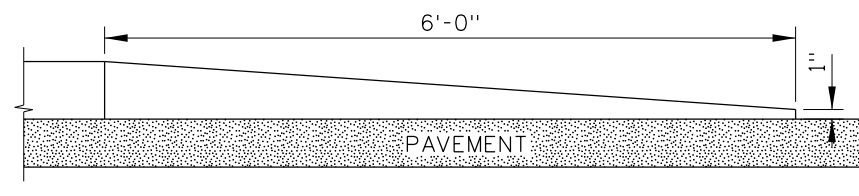
KEY DETAIL
(SEE NOTE NO. 3)



PIN DETAIL
(SEE NOTE NO. 3)



SECTION A-A



CURB TERMINUS DETAIL
(SEE NOTE NO. 5)

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

ORIGINAL SIGNED BY: RYAN D. LANCASTER
DATE ORIGINAL SIGNED: DECEMBER 1, 2014

REVISIONS							
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2	12-90	GB	7	06-05	MSM		
3	09-93	MSM	8	07-10	JAW		
4	12-94	MSM	9	11-14	RDL		
5	12-01	MSM					

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME: 615-1_1114.dgn
DRAWING DATE: APRIL, 1961

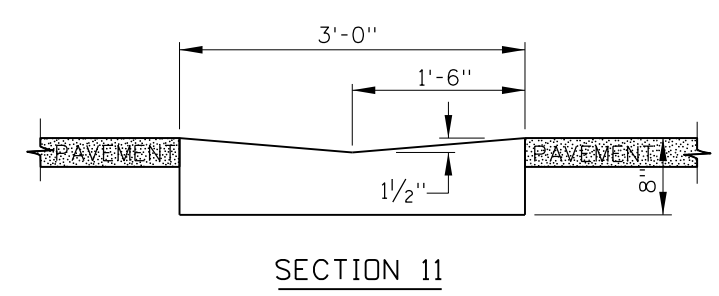
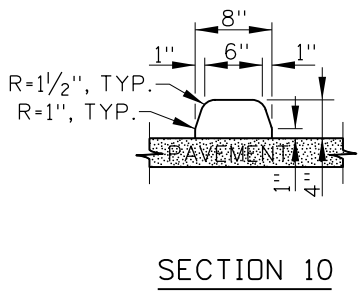
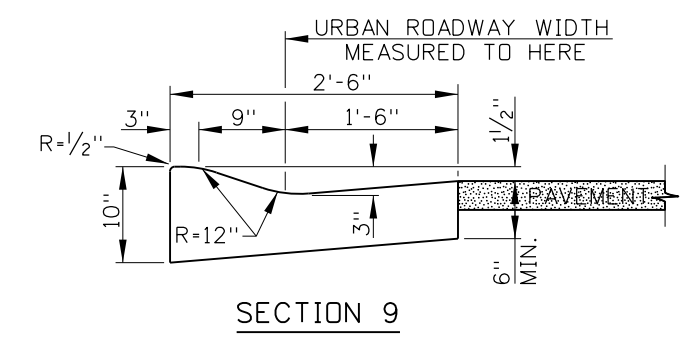
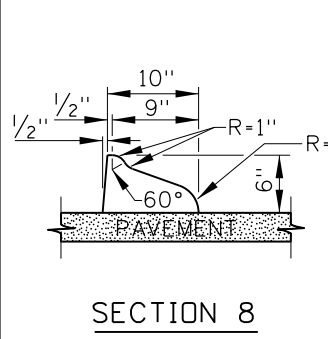
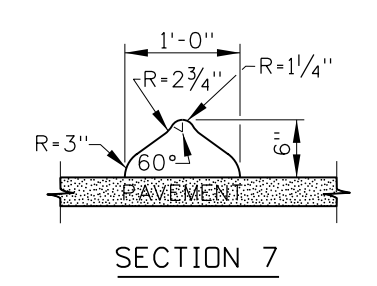
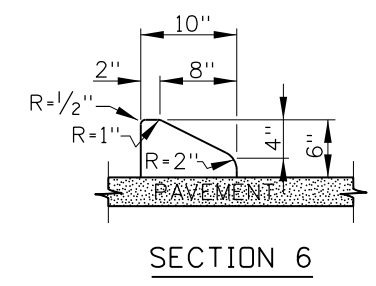
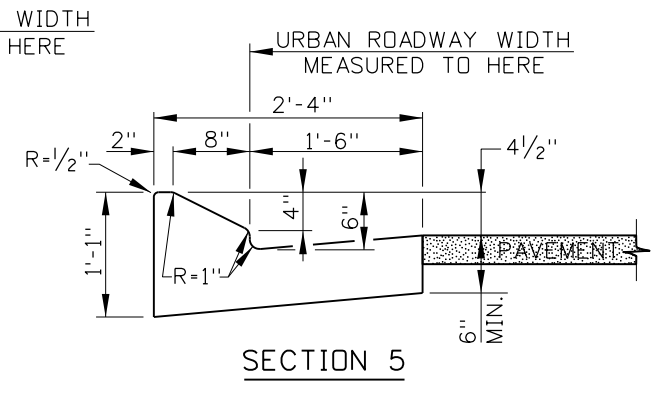
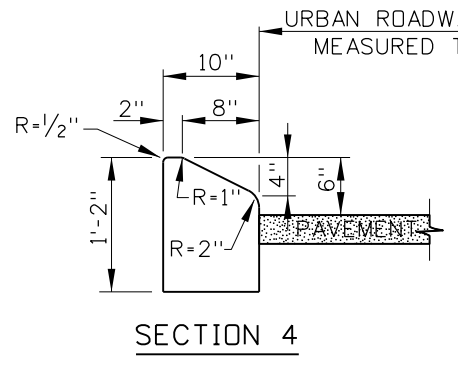
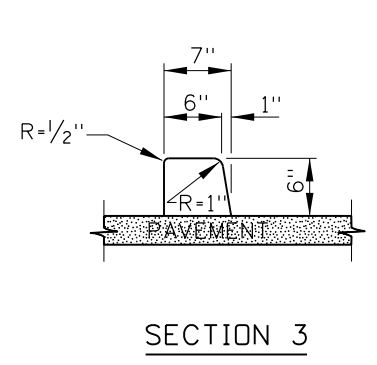
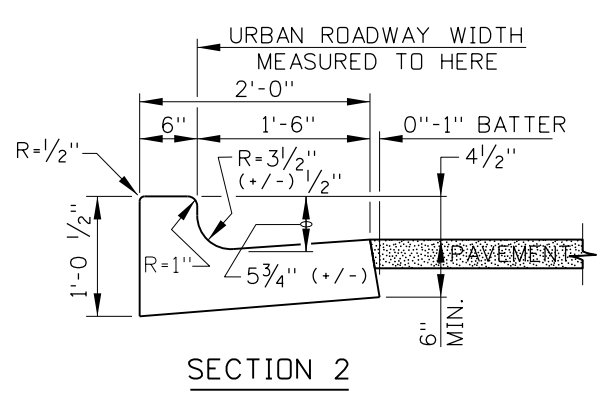
IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: CARL D. MAIN for STANDARDS ENGINEER

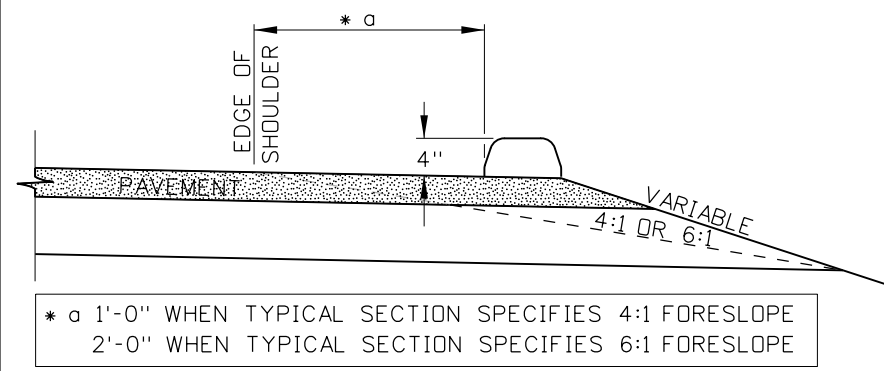
STANDARD DRAWING
CURB AND GUTTER
(SHEET 1 IS FOR USE WITH 2015 OR NEWER SUPPLEMENTAL SPECIFICATIONS)
REQUIRES SHEET 2 OF 2

English
STANDARD DRAWING NO.
615-1
SHEET 1 OF 2



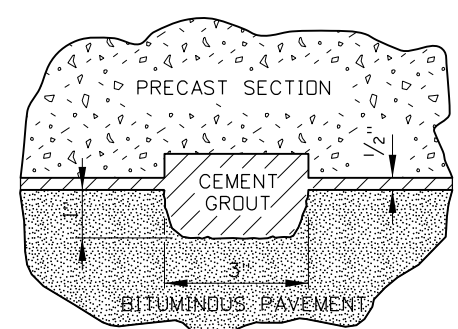
NOTES

1. THERE SHALL BE FOUR TYPES OF CURB, GUTTER, AND TRAFFIC SEPARATORS AS FOLLOWS:
TYPE A SECTIONS SHALL BE CAST-IN-PLACE PORTLAND CEMENT CONCRETE.
TYPE B SECTIONS SHALL BE PRECAST PORTLAND CEMENT CONCRETE.
TYPE C SECTIONS SHALL BE EXTRUDED PORTLAND CEMENT CONCRETE.
TYPE D SECTIONS SHALL BE EXTRUDED ASPHALT CONCRETE.
2. WHERE DIFFERENT CURB SECTIONS CONNECT, PROVIDE A UNIFORM TRANSITION WITH A MINIMUM LENGTH OF 12 TIMES THE LARGEST VARIATION IN CURB DIMENSIONS.
3. WHEN CONCRETE CURBS OR TRAFFIC SEPARATORS ARE PLACED ON TOP OF BITUMINOUS PAVEMENT, A KEY APPROXIMATELY 1" DEEP BY 3" WIDE SHALL BE PLACED AT THE CENTERLINE OF THE SECTION FOR ITS ENTIRE LENGTH. WHEN PRECAST CONCRETE SECTIONS ARE PLACED ON THE PAVEMENT, A KEY APPROXIMATELY 1" DEEP BY 3" WIDE SHALL BE PROVIDED IN THE BOTTOM OF THE SECTION. WHEN BITUMINOUS SECTIONS ARE USED, NO KEY IN THE PAVEMENT WILL BE REQUIRED. CURB PIN DOWELS MAY BE PROVIDED AS AN ALTERNATIVE TO PROVIDING A KEY. THE DOWELS SHALL BE #6 DEFORMED REBAR AND SHALL BE INSTALLED AT A MAXIMUM SPACING OF 5'. THE DOWELS SHALL EXTEND 8" BELOW THE FINISHED PAVEMENT SURFACE AND 4" INTO THE CURB. FOR CURB SECTION 10, THE DOWELS SHALL EXTEND INTO THE CURB TO PROVIDE 1" OF COVER. PRECAST CONCRETE CURBS SHALL HAVE A MINIMUM LENGTH OF 6' WITH 2 DOWELS. ANY SECTION LONGER THAN 6' SHALL HAVE A MINIMUM OF 3 DOWELS. NO PRECAST CONCRETE SECTION SHALL EXCEED 10'.
4. PRECAST OR EXTRUDED CONCRETE CURB AND TRAFFIC SEPARATORS PLACED ON PORTLAND CEMENT SURFACES SHALL BE ATTACHED TO THE SURFACE WITH AN EPOXY BONDING AGENT. NO KEY WILL BE REQUIRED.
5. AT SPECIFIED LOCATIONS, A REQUIREMENT SUCH AS "CONSTRUCT TYPE A-2 CURB AND GUTTER" INDICATES TYPE A CONSTRUCTION AS DEFINED IN THE SPECIFICATIONS AND SECTION 2 AS SHOWN ON THIS DRAWING. A DESIGNATION SUCH AS "CONSTRUCT TYPE B-6 OR C-6 CURB" INDICATES THAT EITHER TYPE B OR TYPE C CONSTRUCTION, AS DEFINED IN THE SPECIFICATIONS, MAY BE USED TO CONSTRUCT SECTION 6.
6. THE TERMINUS ENDS OF CURBS SHALL BE TAPERED DOWN IN THE LAST 6' TO A MAXIMUM OF 1" THICKNESS AT THE EXPOSED END.
7. REFER TO STANDARD DRAWING R-2 WHEN TRANSITIONING AND FLATTENING CURB AND/OR CURB & GUTTER FOR A RAILROAD CROSSING.
8. REFER TO STANDARD DRAWING H-1-B FOR A.D.A. CURB AND GUTTER SECTIONS TO BE USED AT A.D.A. PEDESTRIAN RAMPS.
9. REFER TO STANDARD DRAWING H-1-B FOR AGGREGATE BASE THICKNESS REQUIRED BENEATH CURB AND/OR CURB AND GUTTER SECTIONS.
10. NOT TO SCALE.

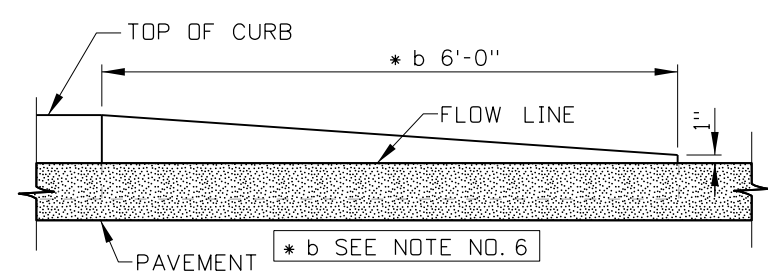


* a 1'-0" WHEN TYPICAL SECTION SPECIFIES 4:1 FORESLOPE
2'-0" WHEN TYPICAL SECTION SPECIFIES 6:1 FORESLOPE

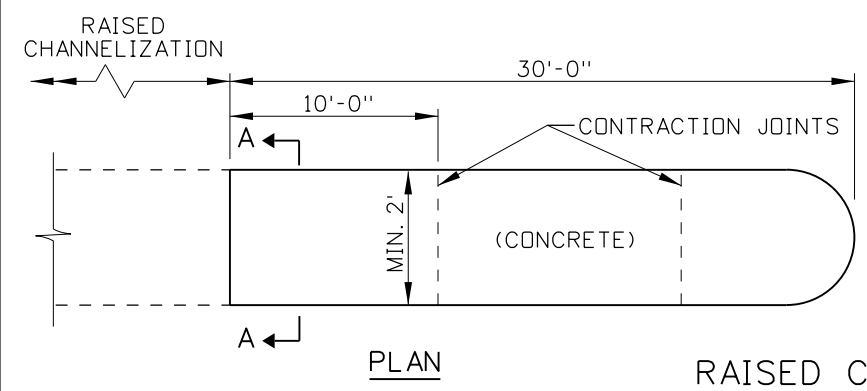
TYPICAL CURB INSTALLATION
(WHEN USED IN CONJUNCTION WITH
GUARDRAIL SEE STD. DWG G-1-A-1)



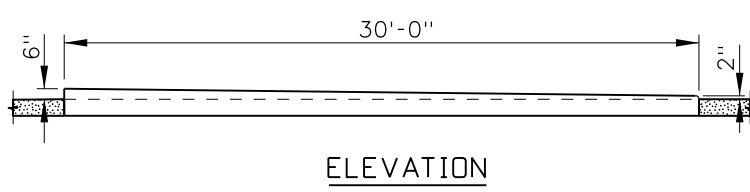
FOR CURB PLACED ON BITUMINOUS PAVEMENT
TYPICAL GROUT JOINT



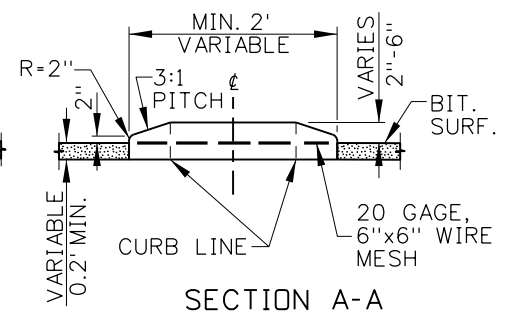
CURB TERMINUS



PLAN



ELEVATION



SECTION A-A

RAISED CHANNELIZATION END TREATMENT

REVISIONS							
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1	02-76		6	12-04	MSM		
2	12-90	GB	7	06-05	MSM		
3	09-93	MSM	8	07-10	JAW		
4	12-94	MSM	9	11-14	RDL		
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DRAWING DATE: APRIL, 1961

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: CARL D. MAIN for
STANDARDS ENGINEER

STANDARD DRAWING
CURB AND GUTTER
(SHEET 2 IS FOR USE WITH 2014 OR OLDER SUPPLEMENTAL SPECIFICATIONS)
REQUIRES SHEET 1 OF 2

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

STANDARD DRAWING NO.
615-1

SHEET 2 OF 2

ORIGINAL SIGNED BY:
RYAN D. LANCASTER
DATE ORIGINAL SIGNED:
DECEMBER 1, 2014